

COUNTY OF SAN MATEO



Reconstruction of Leland Avenue

**(from Palo Alto Way to Frontage Road near Sand
Hill Road)**

Stanford Avenue

**(from Palo Alto Way to Frontage Road near Sand
Hill Road)**

Palo Alto Way

(from Santa Cruz Avenue to Leland Avenue)

December 3, 2015

County of San Mateo
Department of Public Works



Agenda

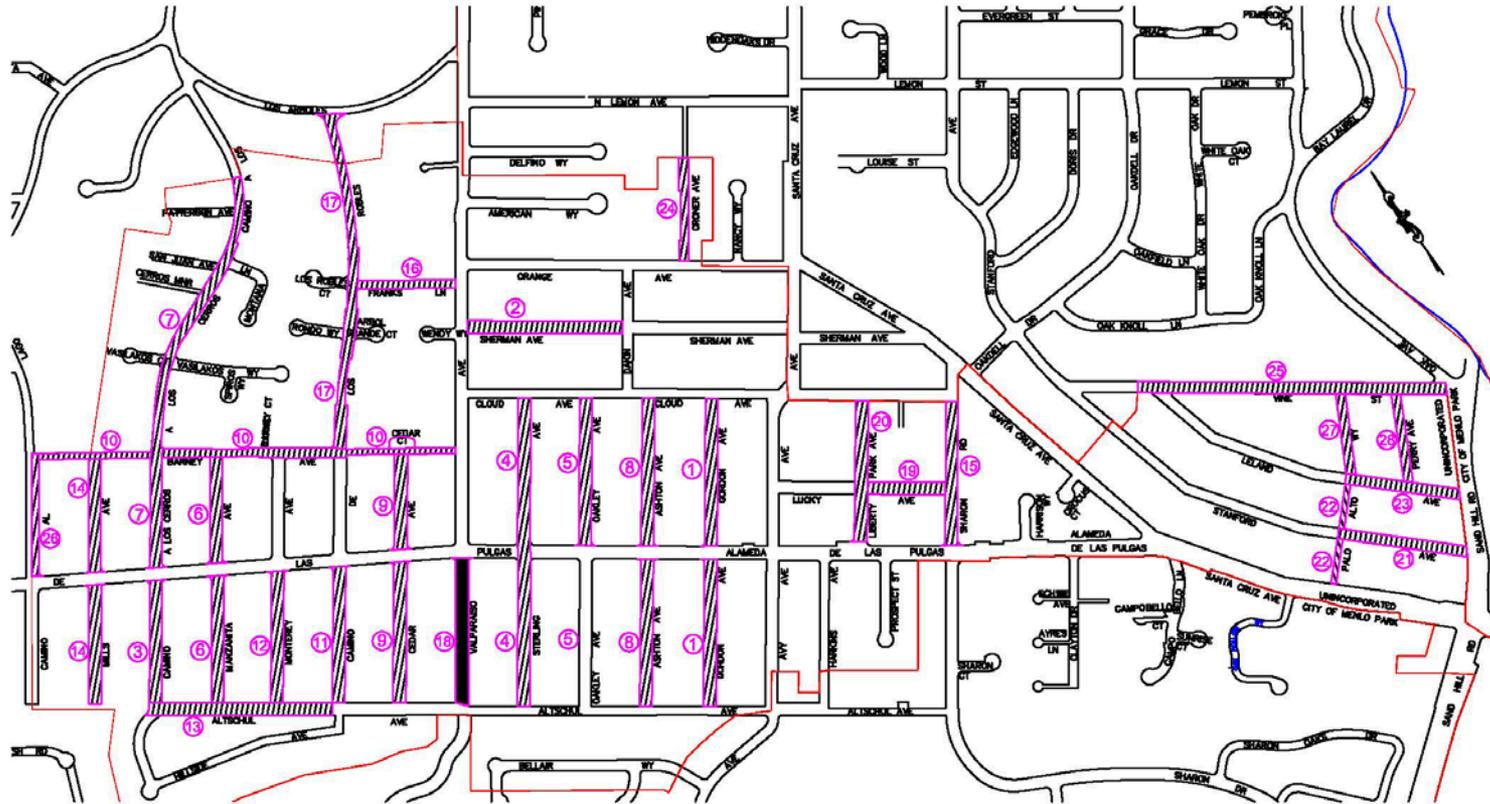
- West Menlo Park Area Road Improvements
- Project Description & Property Owner Survey
- Road Alignment
- Proposed Process and Timeline
- Questions, Comments, and Input



Road Improvement Standards and Priority List

- **1996** – Began development of Road Standards and Improvement Priority List
 - Property owner surveys on unimproved streets and public meeting
- **1997** - Board of Supervisors (Board) adopted Road Standards and Priority List
- **1999 & 2003** – Board adopted modified Road Standards and procedures for determining Road Standards to be used





LEGEND

-  PARTIALLY IMPROVED ROAD
Road with structural section without shoulder treatment, as defined above.
-  UNIMPROVED ROAD
Road with structural section. Existing roadway believed to be comprised of numerous layers of oiled rock and cutback.

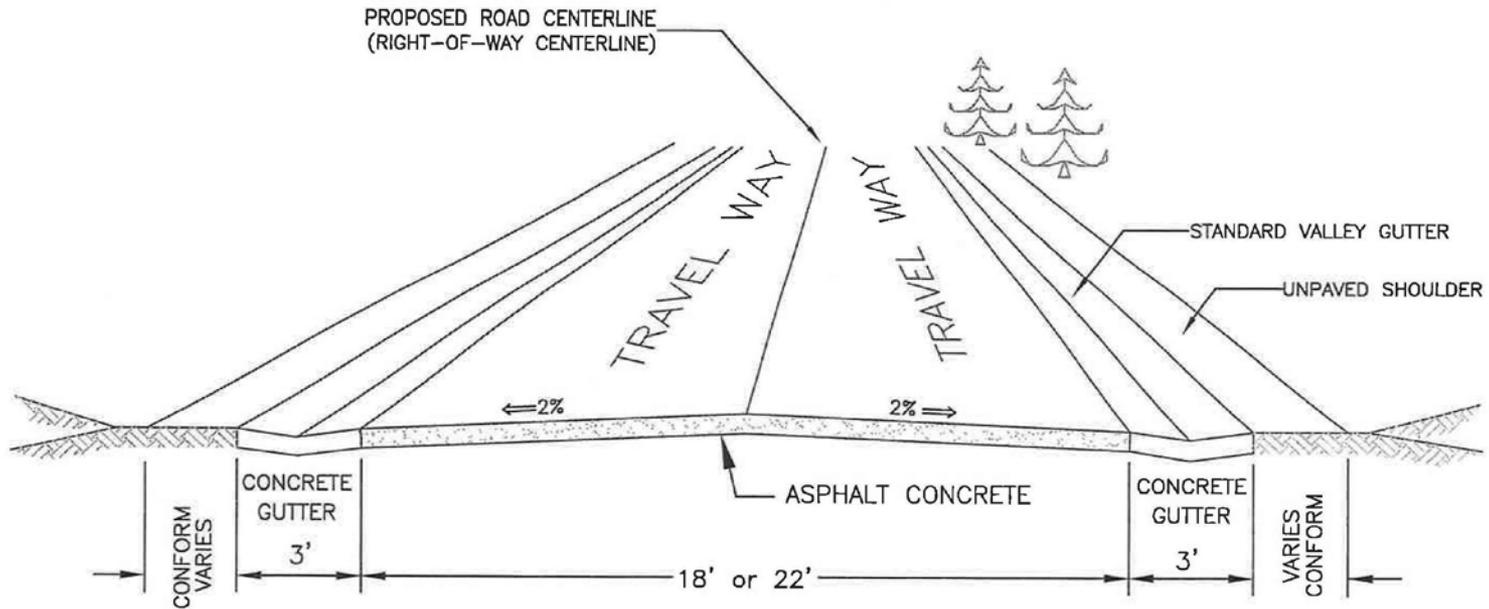
-  CITY / COUNTY LIMITS
-  PRIORITY NUMBER
- A.C. Asphalt Concrete
- s/w Sidewalk
- c/c Curb and Gutter, County Standard
- s/w & c/c Are constructed of concrete unless otherwise noted.



West Menlo Park Area Priority List

Priority No	Street Name	From	To
1	Gordon Avenue	Altschul Avenue	Cloud Avenue
2	Sherman Avenue	Valparaiso Avenue	Dakin Avenue
3	Camino a los Cerros	Altschul Avenue	Alameda De Las Pulgas
4	Sterling Avenue	Altschul Avenue	Cloud Avenue
5	Oakley Avenue	Cloud Avenue	Alameda De Las Pulgas
6	Manzanita Avenue	Altschul Avenue	Barney Avenue
7	Camino a los Cerros	Alameda De Las Pulgas	(past) Patterson Avenue
8	Ashton Avenue	Altschul Avenue	Cloud Avenue
9	Cedar Avenue	Altschul Avenue	Barney Avenue
10	Barney Avenue	Valparaiso Avenue	Camino al Lago
11	Camino De Los Robles	Altschul Avenue	Alameda De Las Pulgas
12	Monterey Avenue	Altschul Avenue	Alameda De Las Pulgas
13	Altschul Avenue	Camino De Los Robles	Camino a Los Cerros
14	Mills Avenue	Barney Avenue	End
15	Sharon Road	Alameda De Las Pulgas	Cloud Avenue
16	Franks Lane	Camino De Los Robles	Valparaiso Avenue
17	Camino De Los Robles	Barney Avenue	Camino Por Los Arboles
18	Valparaiso Avenue	Altschul Avenue	Alameda De Las Pulgas
19	Lucky Avenue	Liberty Park Avenue	Sharon Road
20	Liberty Park Avenue	Alameda De Las Pulgas	Cloud Avenue
21	Stanford Avenue	Palo Alto Way	Sand Hill Road
22	Palo Alto Way	Leland Avenue	Santa Cruz Avenue
23	Leland Avenue	Palo Alto Way	Sand Hill Road
24	Croner Avenue	Orange Avenue	Unincorporated Limits
25	Vine Street	Leland Avenue	Sand Hill Road
26	Camino al Lago	Alameda De Las Pulgas	Barney Avenue
27	Palo Alto Way	Vine Street	Leland Avenue
28	Perry Avenue	Vine Street	Leland Avenue





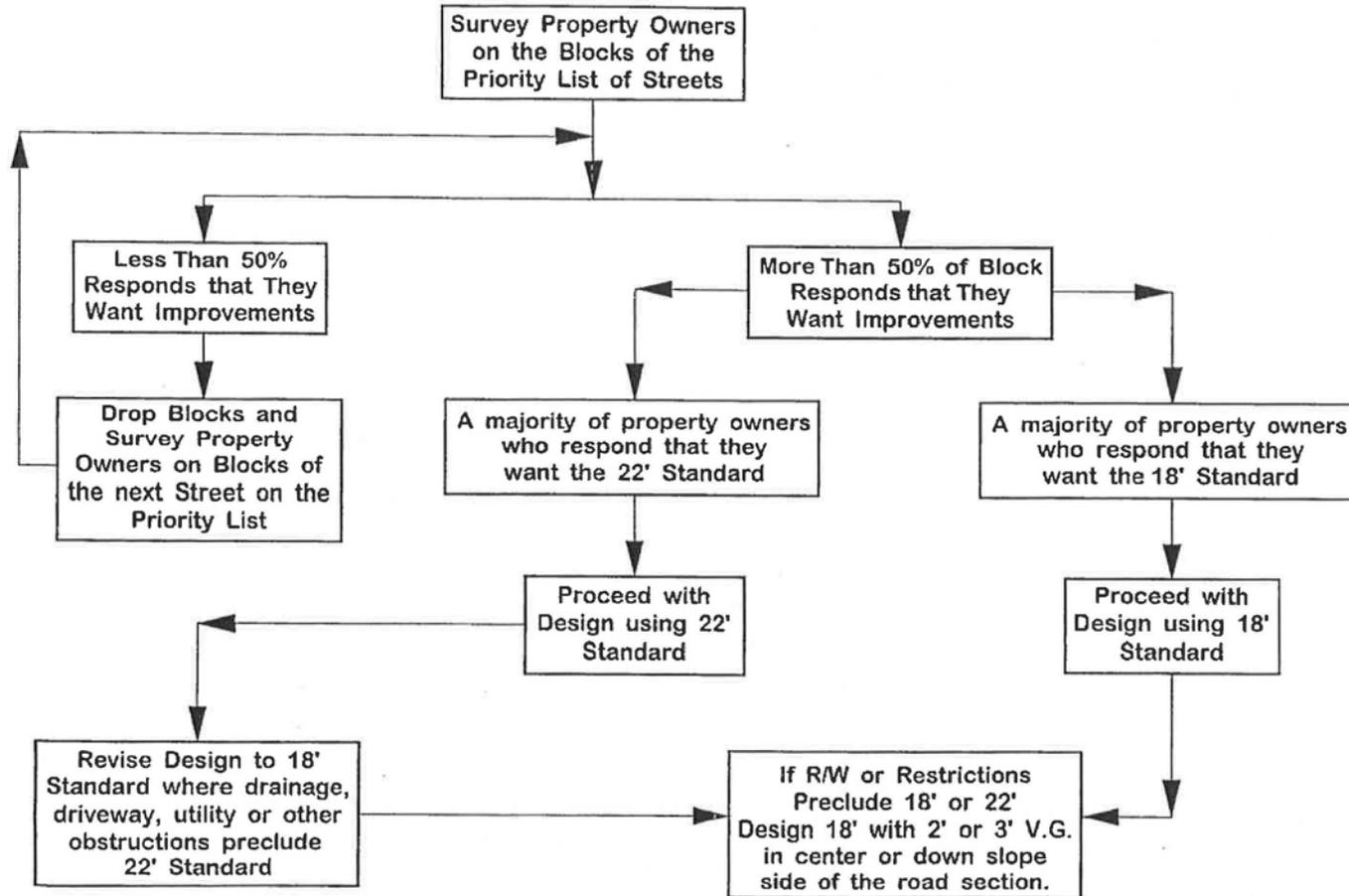
18 or 22-foot-wide Travel Way with 3-foot-wide Valley Gutter Typical Section

NO SCALE

West Menlo Park Area Road Standard

Note: Board Resolution No. 065764, dated 1/07/03 allowed for “the installation of a single valley gutter in the center of the road or on one side of the road where topography or limited right-of-way precludes the construction of valley gutters on both sides of the road.”

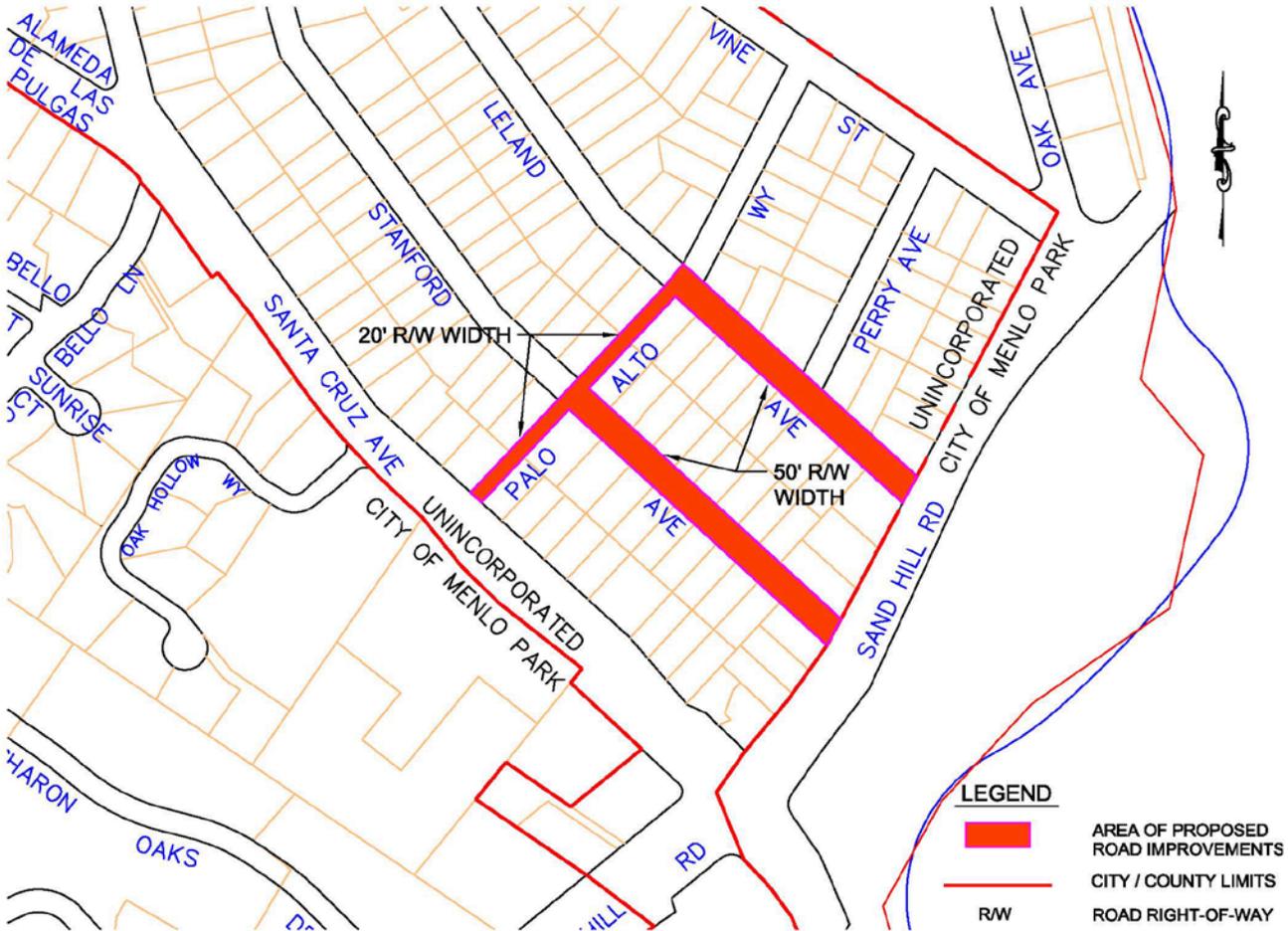
Design Decision Tree



* A Block is the length of road between cross streets.



Project Location



Project Description

- Based on the Board's policy, the improvements are to be centered in the road right-of-way
- Leland Avenue and Stanford Avenue - Reconstruct road with 18 feet or 22 feet of pavement with 3-foot wide valley gutters
- Palo Alto Way - Reconstruct road to 18 feet with 3-foot wide valley gutter located at center of road
- Policy allows for a reduction in width to reduce conflicts with existing conditions



Survey (Leland & Stanford Avenues)

Assessors Parcel Number (APN) located on top portion of mailing label on the envelope:

— — — - — — — - — — —

Your input is very important to us. Please take the time to fill out the form and mail it to us by **Wednesday, December 16, 2015**. **Street improvements will not be constructed unless at least fifty percent (50%) of all the property owners on a given block (based on front footage) indicate that improvements are desired.**

The following should be the minimum standard used for reconstructing the street adjacent to my property. (Please check only one):

- Option 1** – Do nothing. Maintain as is.
- Option 2** – Reconstruct road with 18 feet of pavement with 3-foot wide valley gutters.
- Option 3** – Reconstruct road with 22 feet of pavement with 3-foot wide valley gutters.

Note:

Should at least fifty percent (50%) of all the property owners on a given block vote for improvements, the option that receives the majority of the vote (based on front footage of all the property owners) will be constructed.



Survey (Palo Alto Way)

Assessors Parcel Number (APN) located on top portion of mailing label on the envelope:

_____ - _____ - _____

Your input is very important to us. Please take the time to fill out the form and mail it to us by **Wednesday, December 16, 2015**. **Street improvements will not be constructed unless at least fifty percent (50%) of all the property owners on a given block (based on front footage) indicate that improvements are desired.**

The following should be the minimum standard used for reconstructing the street adjacent to my property. (Please check only one):

- Option 1** – Do nothing. Maintain as is.
- Option 2** – Reconstruct road to 18-foot wide road with a 2-foot wide valley gutter at center.
- Option 3** – Reconstruct road to 18-foot wide road with a 3-foot wide valley gutter at center.

Note:

Should at least fifty percent (50%) of all the property owners on a given block vote for improvements, the option that receives the majority of the vote (based on front footage of all the property owners) will be constructed.

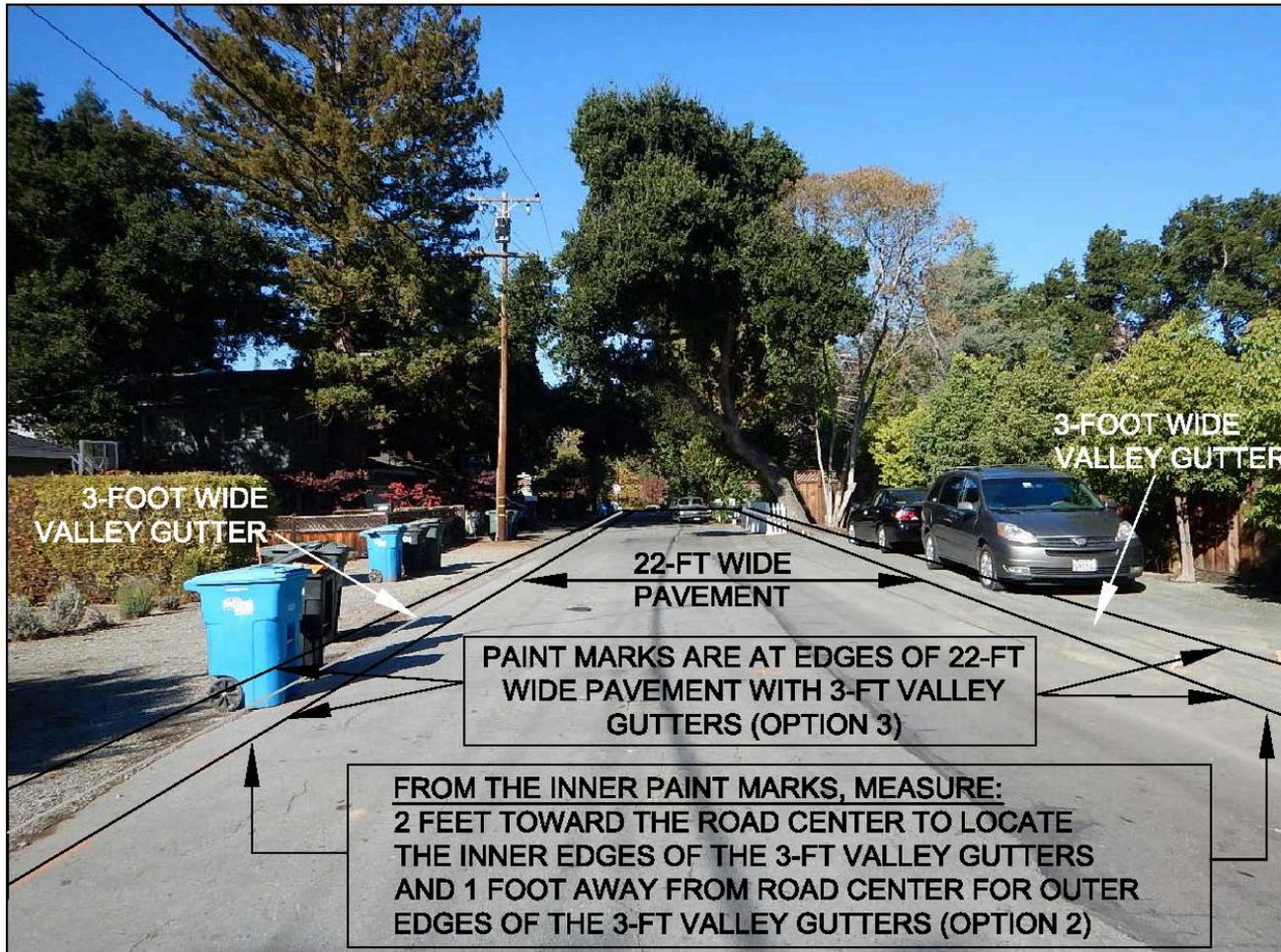


Leland Avenue (Option 2 and 3)

OPTION 2 - RECONSTRUCT ROAD WITH 18-FOOT WIDE PAVEMENT WITH 3-FOOT WIDE VALLEY GUTTERS

OPTION 3 - RECONSTRUCT ROAD WITH 22-FOOT WIDE PAVEMENT WITH 3-FOOT WIDE VALLEY GUTTERS

(Pictorial Not Illustrated To Scale)

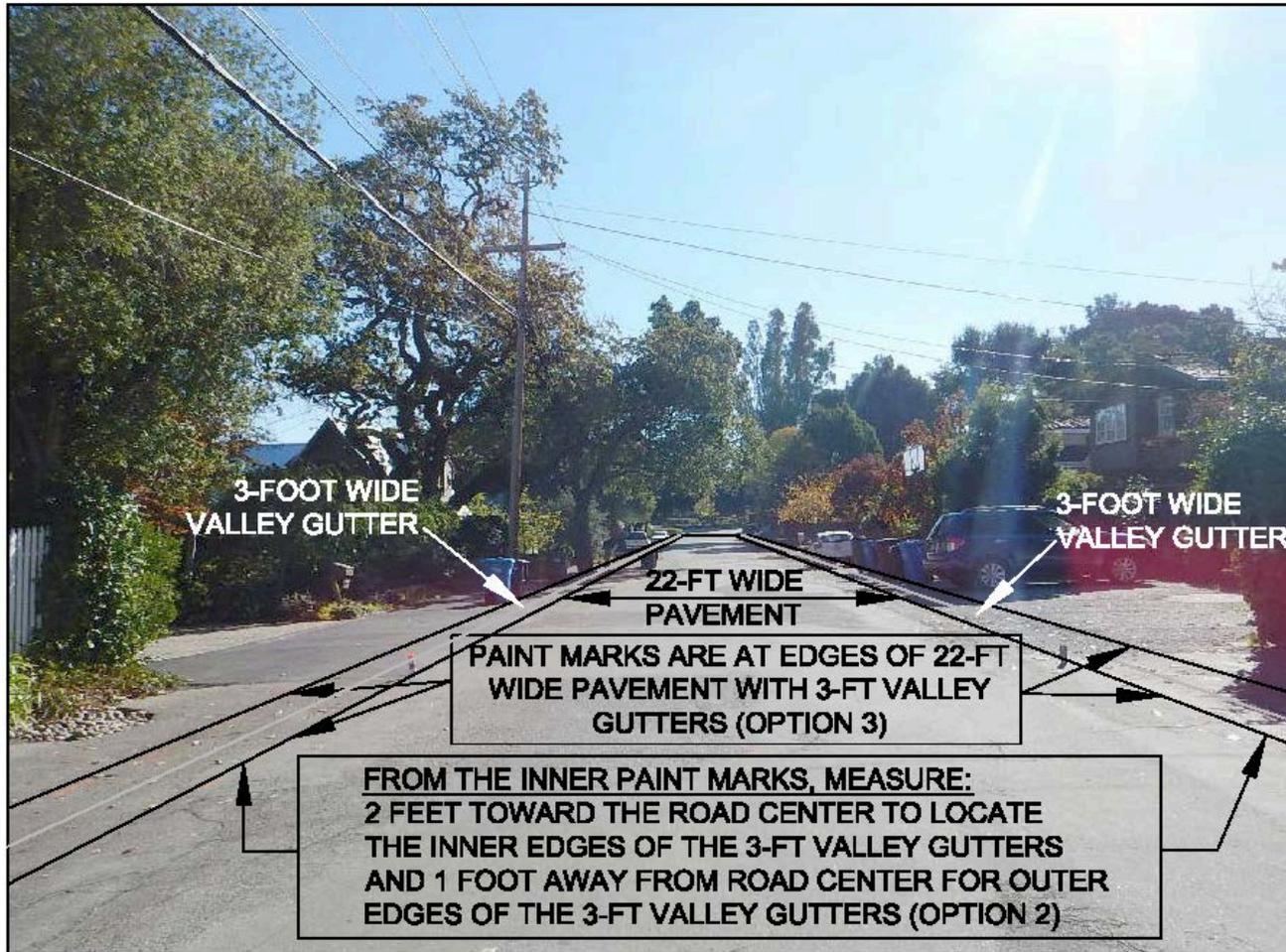


Stanford Avenue (Option 2 and 3)

OPTION 2 - RECONSTRUCT ROAD WITH 18-FOOT WIDE PAVEMENT WITH 3-FOOT WIDE VALLEY GUTTERS

OPTION 3 - RECONSTRUCT ROAD WITH 22-FOOT WIDE PAVEMENT WITH 3-FOOT WIDE VALLEY GUTTERS

(Pictorial Not Illustrated To Scale)



Palo Alto Way (Option 2 and 3)

OPTION 2 - RECONSTRUCT ROAD TO 18-FOOT WIDE ROAD WITH 2-FOOT WIDE VALLEY GUTTER

OPTION 3 - RECONSTRUCT ROAD TO 18-FOOT WIDE ROAD WITH 3-FOOT WIDE VALLEY GUTTER

Pictorial Not Illustrated To Scale)



Existing Conditions – Leland Avenue (from Perry Avenue to Palo Alto Way)



Existing Conditions – Stanford Avenue (from Sand Hill Road to Palo Alto Way)



Existing Conditions – Palo Alto Way (from Stanford Avenue to Leland Avenue)



How do the markings match up with the existing conditions?



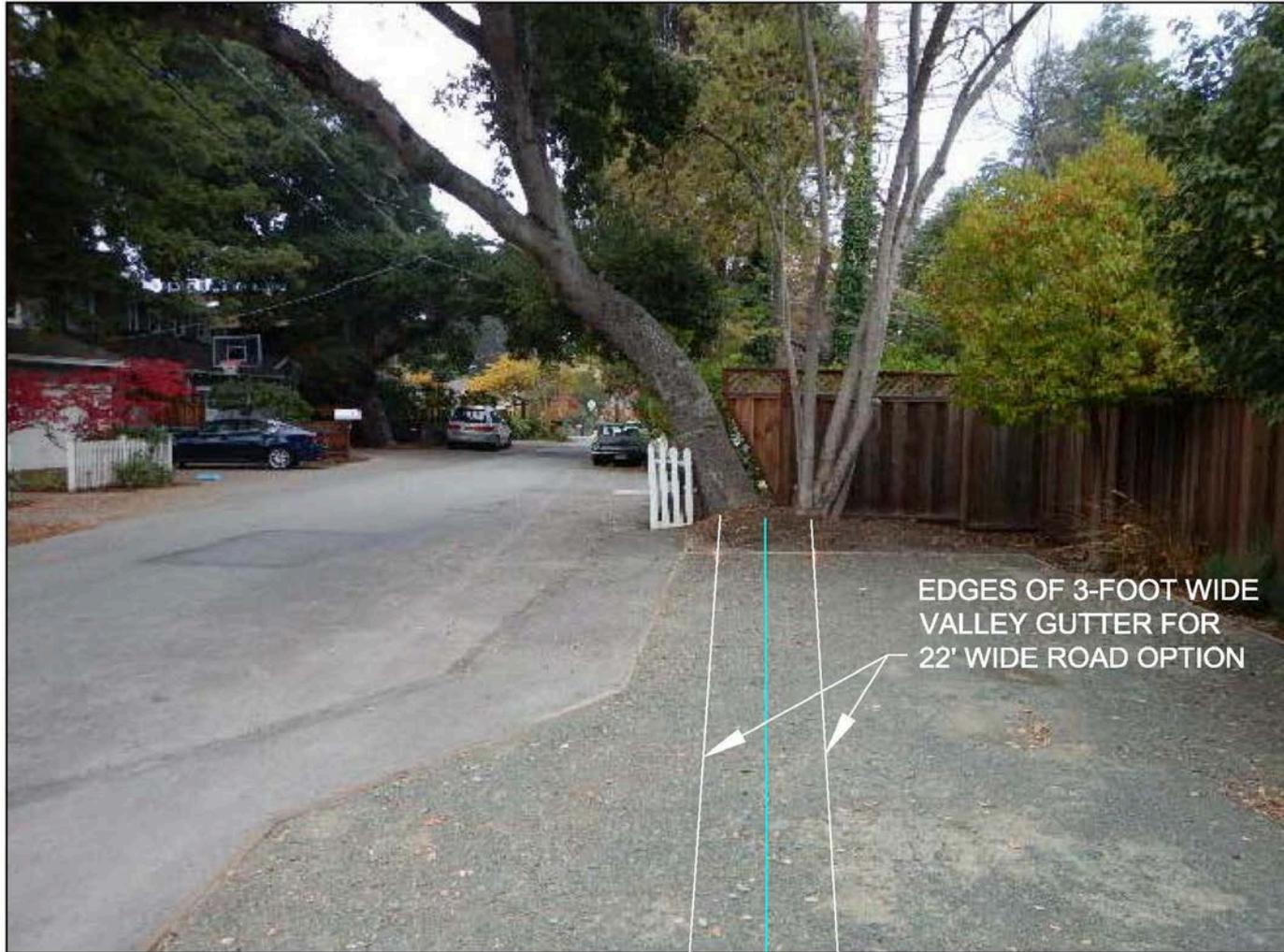
Leland Avenue (from Perry Avenue to Sand Hill Road)



Leland Avenue (from Perry Avenue to Sand Hill Road)



Leland Avenue (from Perry Avenue to Palo Alto Way)



Stanford Avenue

(from Palo Alto Way to Sand Hill Road)



Stanford Avenue (from Palo Alto Way to Sand Hill Road)



EDGES OF 3-FT WIDE
VALLEY GUTTER FOR
22' WIDE ROAD OPTION

Palo Alto Way (from Stanford Avenue to Santa Cruz Avenue)



Palo Alto Way (from Stanford Avenue to Santa Cruz Avenue)



Palo Alto Way (from Leland Avenue to Stanford Avenue)



Design Issues Based on Paint Markings

(Leland Avenue and Stanford Avenue - 22' pavement and 3' valley gutters - centered in road right of way)

- Some driveways might become steeper than the existing driveways (20% maximum slope per County standard)
- Existing drainage features not aligned with proposed valley gutters
- Impacts to existing landscaping/shoulder area/fences/trees



Design Issues Based on Paint Markings

(Palo Alto Way - 18' wide road and 3' valley gutter located at center of road - centered in road right of way)

- Some driveways might become steeper than the existing driveways (20% maximum slope per County standard)
- Impacts to existing landscaping/shoulder area/tree
- The distance from edge of pavement to property line will be 1-foot if 18' wide road is constructed.



Proposed Process & Timeline

- **December 2015**: Meet with property owners and conduct property owner survey
- **January 2016**: Determine if there is a project and standard(s) to be used
- **Spring 2016**: Develop project scope and design project
- **Late Spring 2016**: Advertise and bid out project
- **Summer 2016**: Project construction



Project Funding

- Construction of the roadway and valley gutters and minor work to match up to the valley gutters (driveways and shoulder area).
- Work outside of project scope:
 - Landscaping, shoulder and driveway work beyond the required project limit
 - If Property Owners have been contemplating sewer lateral work, completing it prior to roadway work would be advised. Property owners are responsible for maintenance and repair of the lateral from house to the sewer main.



Questions, Comments, and Input

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